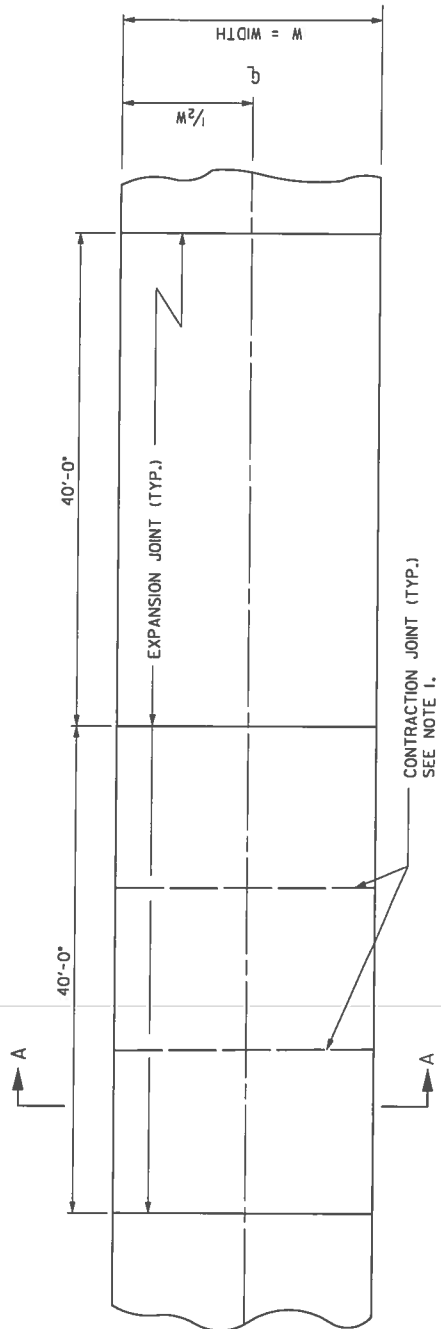


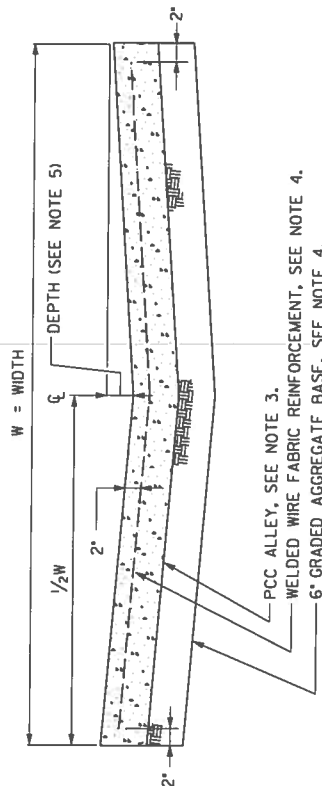
ATTACHMENT J.3 DDOT STANDARD
DETAILS AND DRAWINGS

DDOT STANDARD DETAILS AND DRAWINGS (29 Pages)

- TRENCH REPAIR DETAIL (1 PAGE)
- ALLEY AND ALLEY ENTRANCE DETAILS (3 PAGES)
- ISLAND AND MEDIAN DETAILS (4 PAGES)
- PCC BUS PAD DETAIL (1 PAGE)
- SIDEWALK DETAILS (5 PAGES)
- CURB AND GUTTER DETAILS (4 PAGES)
- WHEELCHAIR/BICYCLE RAMP DETAILS (5 PAGES)
- STEEL DRIVE POST DETAIL (1 PAGE)
- TRAFFIC CALMING (2 PAGES)
- TREE PROTECTION DETAILS (3 PAGES)



PLAN



SECTION A-A

NOTES:

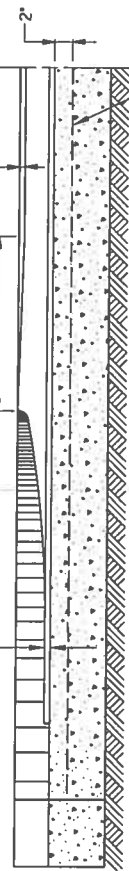
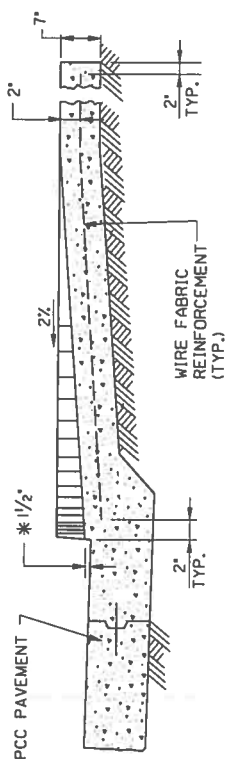
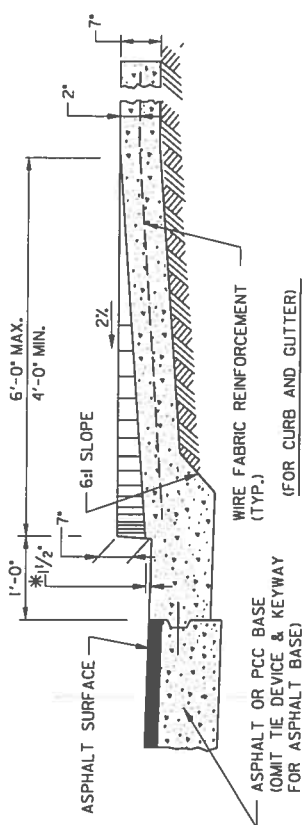
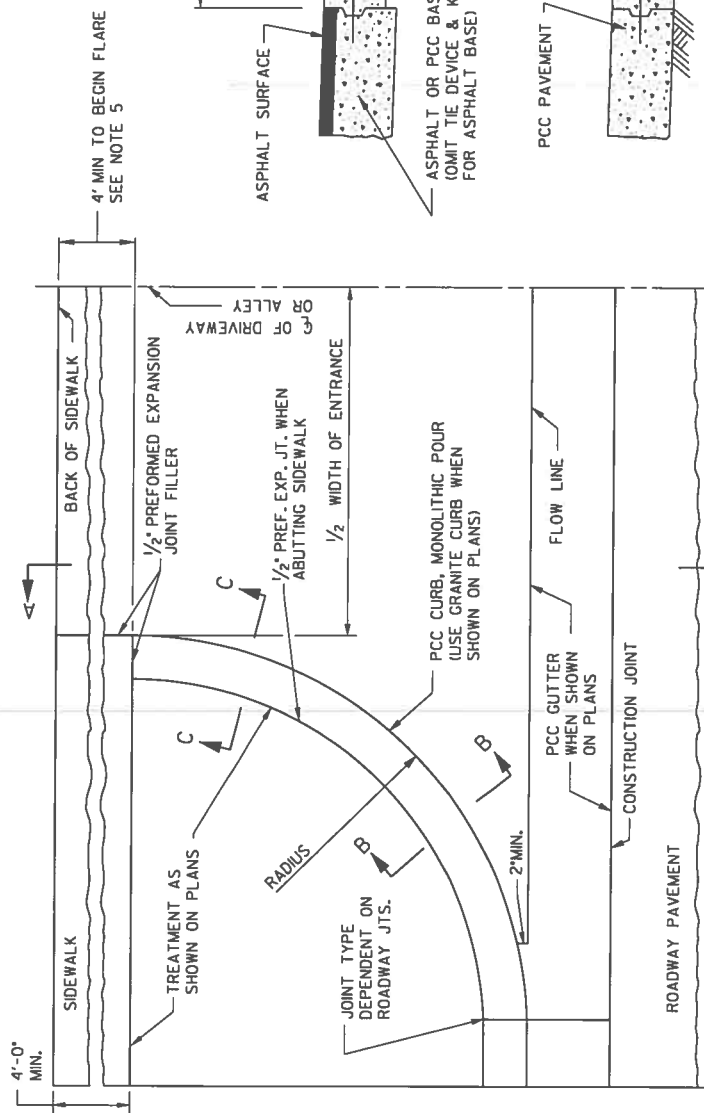
1. PLACE TRANSVERSE EXPANSION JOINTS AT APPROX. 40 FT. INTERVALS AND AT END OF ALLEY WHICH ABUTS CONCRETE PAVING. PLACE CONTRACTION JOINTS AT APPROX. 13 FT. INTERVALS. THESE CONTRACTION JOINTS SHALL CONSIST OF A GROOVE FORMED WITH A JOINTING TOOL WITH A BLADE PROJECTION 1/3 THE DEPTH OF PAVING. AS AN ALTERNATE METHOD, SAWN JOINTS WILL BE PERMITTED AND WILL BE DONE IN ACCORDANCE WITH SECTION 501.14(D) OF THE DOT STANDARD SPECIFICATIONS.
2. PLACE LONGITUDINAL EXPANSION JOINTS BETWEEN THE ALLEY SLAB AND PERMANENT STRUCTURES ALONG THE SIDES OF ALLEY.
3. DEPTH OF ALLEY PAVING IS 7"-8" IN COMMERCIAL AREAS AND 6" IN RESIDENTIAL AREAS.
4. WIRE FABRIC REINFORCEMENT AND GRADED AGGREGATE BASE SHALL BE AS REQUIRED BY THE CONTRACT DOCUMENTS. WIRE FABRIC SHALL NOT EXTEND ACROSS EXPANSION JOINTS.
5. STANDARD DEPTH (DISH) FOR ALLEY IS 4" - 6". MAX. DEPTH IS 11". MIN. DEPTH IS 1". DEPTHS LESS THAN 4" OR GREATER THAN 6" MUST BE APPROVED BY THE ENGINEER.
6. ALLEY TO BE BUILT TO APPROVED ALLEY GRADE.

DATE	APPR.	ISSUED:	REFERENCE
REVIS	REVISED		
RECOMMENDED:		APPROVED:	
DEPUTY CHIEF ENGINEER		CHIEF TRANSPORTATION ENGINEER	

PCC ALLEY

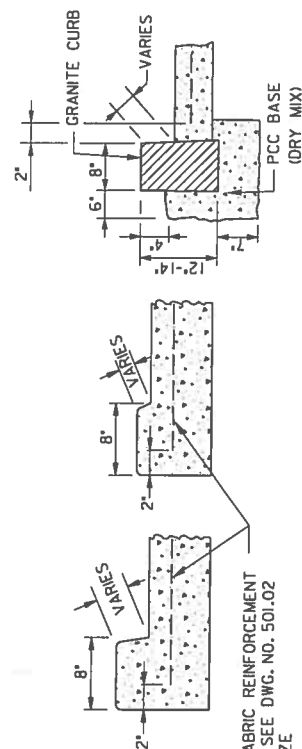
d. DISTRICT OF COLUMBIA
DEPARTMENT OF TRANSPORTATION

DWG. NO. 503.01



SECTION ELEVATION AT GUTTER LINE

- NOTES:
- SECTION ELEVATION AT GUTTER LINE
1. DRIVEWAY RADIUS = 6 FT. (UNLESS OTHERWISE SHOWN ON CONTRACT PLANS).
 2. ALLEY RADIUS = 10 FT. (UNLESS OTHERWISE SHOWN ON CONTRACT PLANS).
 3. 7 IN. PCC USED (UNLESS OTHERWISE SHOWN ON CONTRACT PLANS).
 4. USE GRANITE CURB RETURNS WHERE EXISTING. SEE GRANITE CURB DETAILS WHEN USED.
 5. TO INSURE CONFORMANCE WITH ADA GUIDELINES WHERE ALLEY AND DRIVEWAY ENTRANCES ABOUT SIDEWALK. THE ENTRANCES, INCLUDING ANY INTERVENING CURB RETURN, SHALL BE FLUSH WITH THE ADJACENT SIDEWALK A MINIMUM OF 4 FT. FORWARD FROM THE BACK OF SIDEWALK.
 6. ELIMINATE LIP ON ALLEY ENTRANCE.



SECTION B-B

SECTION C-C

SECTION A-A

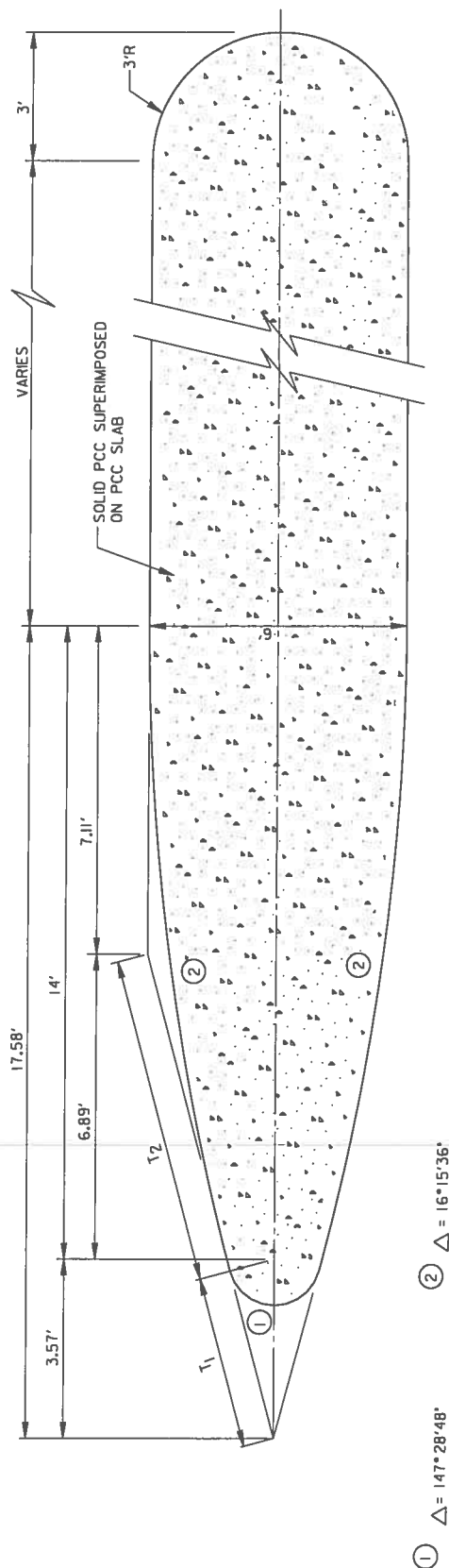
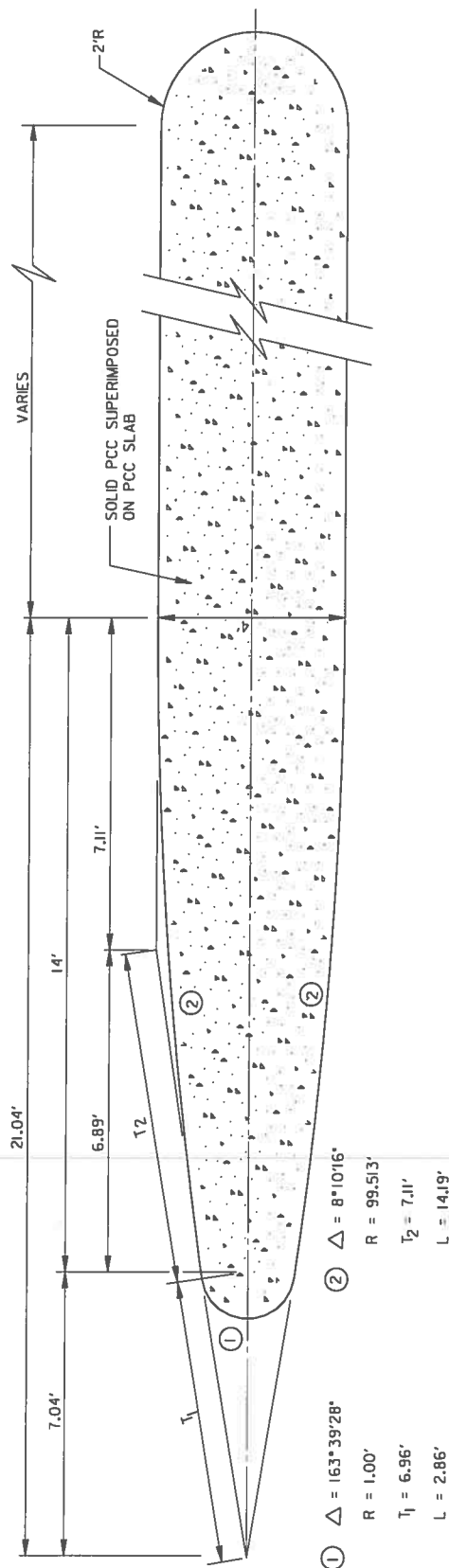
ALLEY-DRIVEWAY ENTRANCE WITH CURB RETURNS TYPE "A"

DISTRICT OF COLUMBIA
DEPARTMENT OF TRANSPORTATION

DWG. NO. 504.01

[illegible]





NOTES:

1. WHERE ISLANDS EXTEND ACROSS CROWDWAYS, CURB CUTS FOR HANDICAPPED SHALL BE CONSTRUCTED AS PART OF THE ISLANDS.
2. ALL DIMENSIONS SHOWN ON THIS DRAWING ARE APPLICABLE TO A 90° CROSSING INTERSECTION. THESE DIMENSIONS SHALL BE ADJUSTED ACCORDINGLY FOR A SKEW CROSSING INTERSECTION.

DIRECTIONAL ISLANDS

DISTRICT OF COLUMBIA
DEPARTMENT OF TRANSPORTATION

DWG. NO. 601.01

[illegible]



*** VARY TO LINE UP WITH CROSSWALKS**



*** VARY TO LINE UP WITH CROSSWALKS**

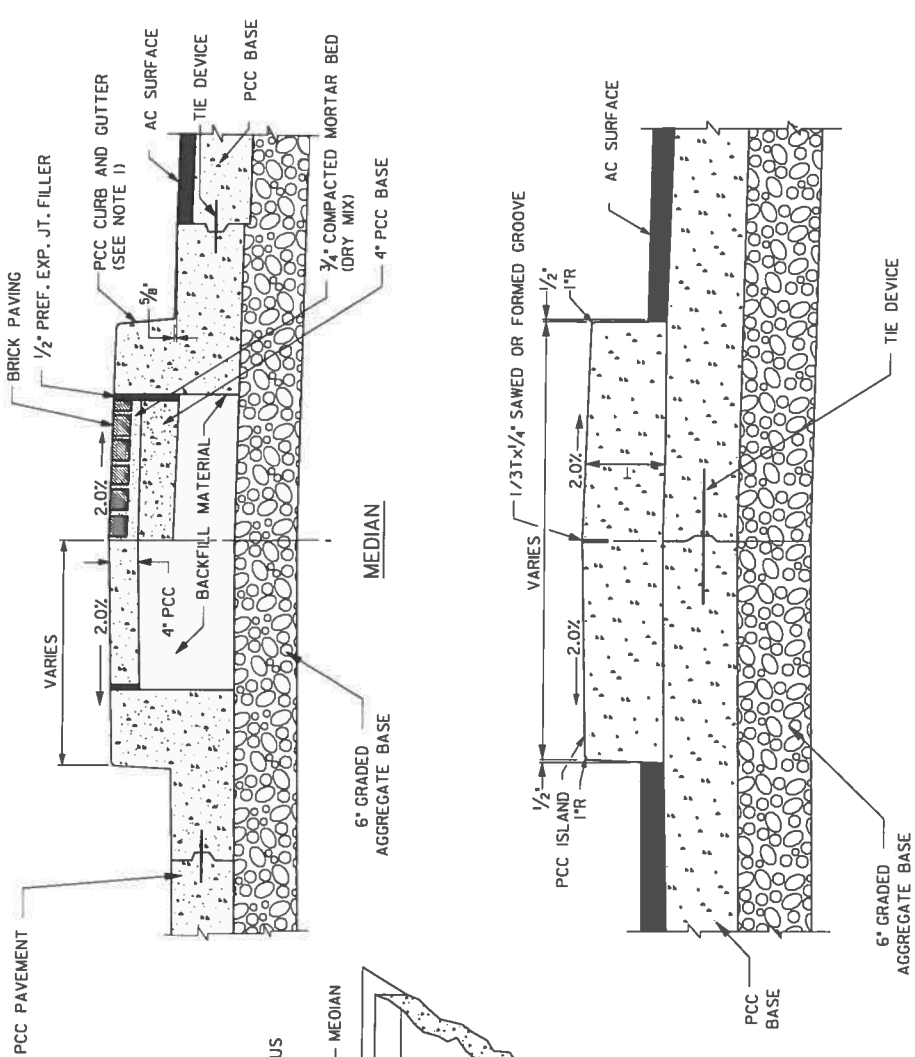
- NOTES:
1. WHERE ISLANDS EXTEND ACROSS CROSSWALKS, CURB CUTS FOR HANDICAPPED SHALL BE CONSTRUCTED AS PART OF THE ISLANDS.
 2. ALL DIMENSIONS SHOWN ON THIS DRAWING ARE APPLICABLE TO A 90° CROSSING INTERSECTION. THESE DIMENSIONS SHALL BE ADJUSTED ACCORDINGLY FOR A SKEWED CROSSING INTERSECTION.

	Δ	R	T	L
①	14° 44' 59"	150'	19.41'	38.61'
②	14° 44' 59"	150'	19.41'	38.61'
③	6° 41' 13"	150'	8.76'	17.51'

DIRECTIONAL ISLANDS

DISTRICT OF COLUMBIA
DEPARTMENT OF TRANSPORTATION

DWG. NO. 601.02



SUPERIMPOSED DIRECTIONAL ISLAND

MEDIAN & ISLAND OPENINGS

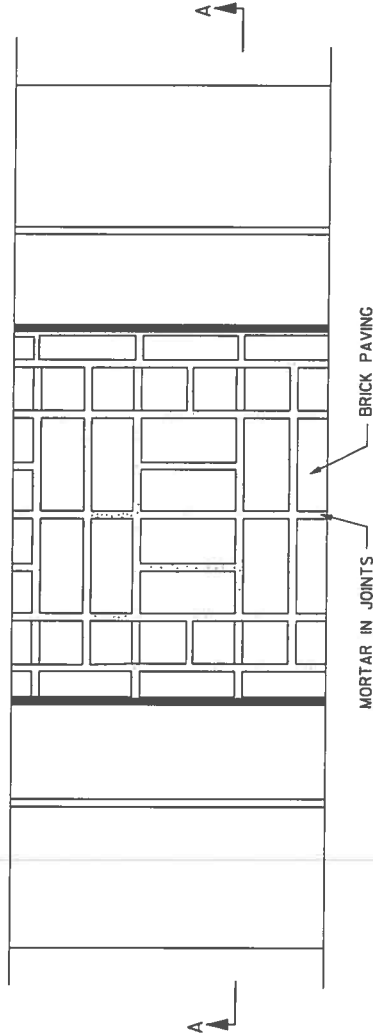
NOTES:

1. CURBS MAY BE GRANITE, SEE DDOT STANDARD DRAWING NO. 609.02.
2. MEDIAN TREATMENT AS SHOWN ON THE CONTRACT PLANS.
3. CURB HEIGHTS AS SHOWN ON THE CONTRACT PLANS. 7" CURB REVEAL TYPICAL.
4. MIN. 2.00% CROSS SLOPE OR AS NOTED ON THE CONTRACT PLANS.
5. 2'-0" MIN. OR AS SHOWN ON THE CONTRACT PLANS.

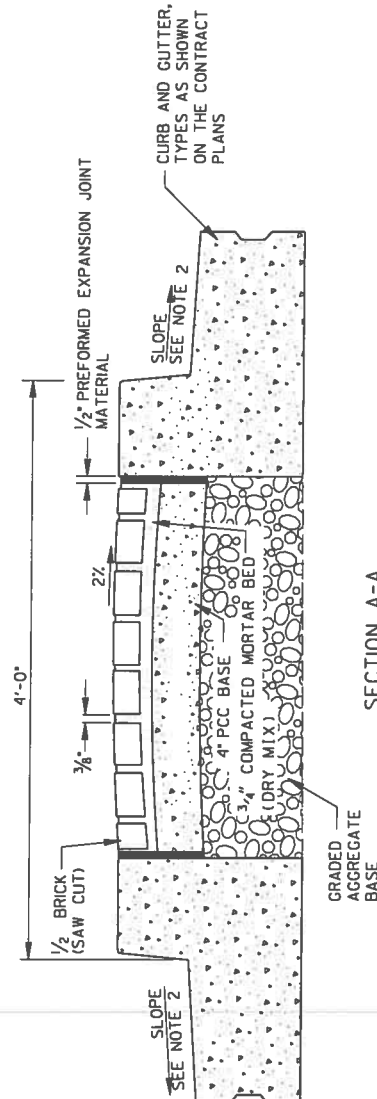
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**MEDIAN AND PEDESTRIAN
REFUGE ISLAND DETAIL**

d. DISTRICT OF COLUMBIA
DEPARTMENT OF TRANSPORTATION



PLAN



SECTION A-A

NOTES:

1. PATTERN OF BRICK SHALL BE AS SHOWN UNLESS INDICATED OTHERWISE ON THE CONTRACT PLANS OR IN THE SPECIAL PROVISIONS.
2. LDW SIDE 1" PER FT. TOWARDS CURB. HIGH SIDE 5/8" PER FT. AWAY FROM CURB.

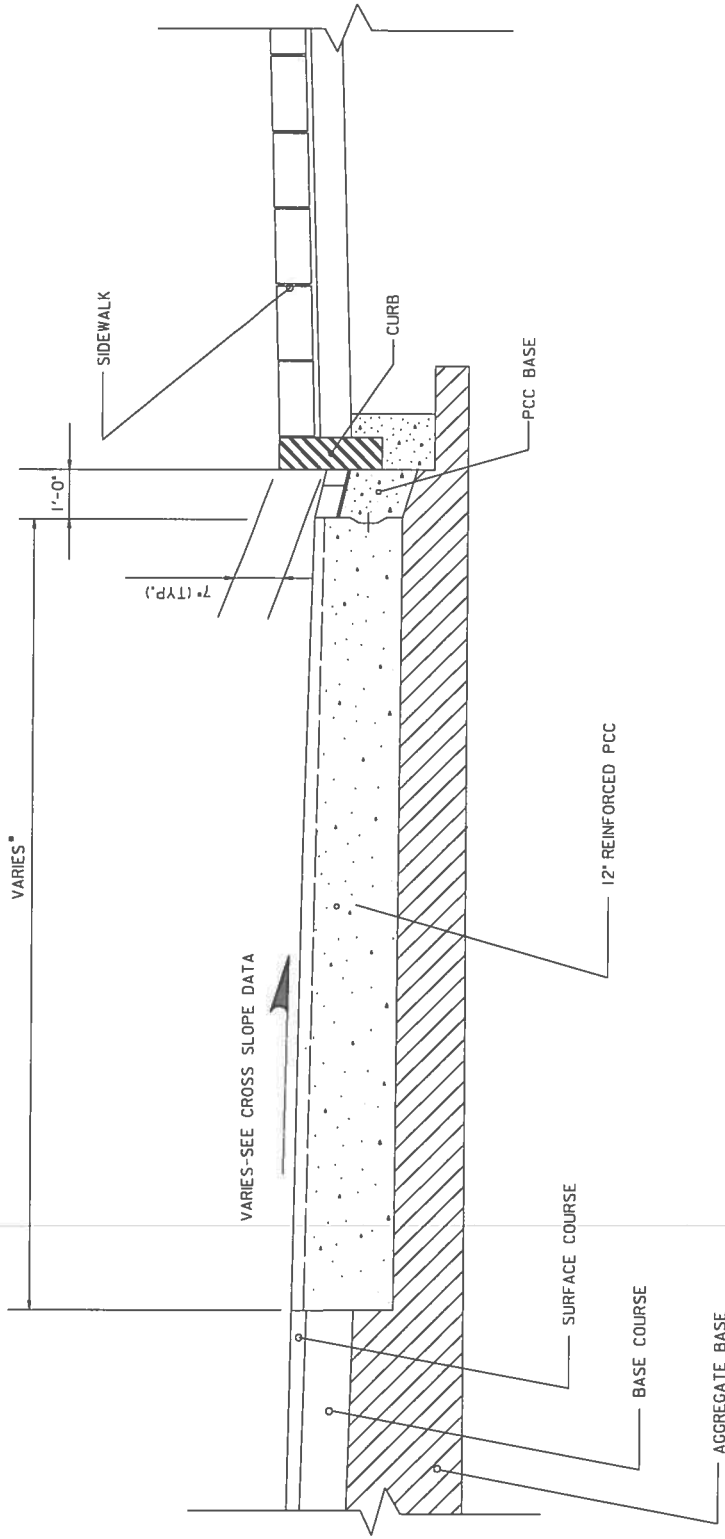
BRICK PATTERN FOR MEDIAN WALK

DATE	APPR. REVISED	ISSUED:	REFERENCE	RECOMMENDED: <i>[Signature]</i> DEPUTY CHIEF ENGINEER
				APPROVED: <i>[Signature]</i> CHIEF TRANSPORTATION ENGINEER

BRICK MEDIAN

d. DISTRICT OF COLUMBIA
DEPARTMENT OF TRANSPORTATION

DWG. NO. 601.04



* WIDTH DEPENDS ON ROADWAY CONFIGURATION.
CONSULT WITH DDOT OFFICE OF MASS TRANSIT.

d. DISTRICT OF COLUMBIA
DEPARTMENT OF TRANSPORTATION

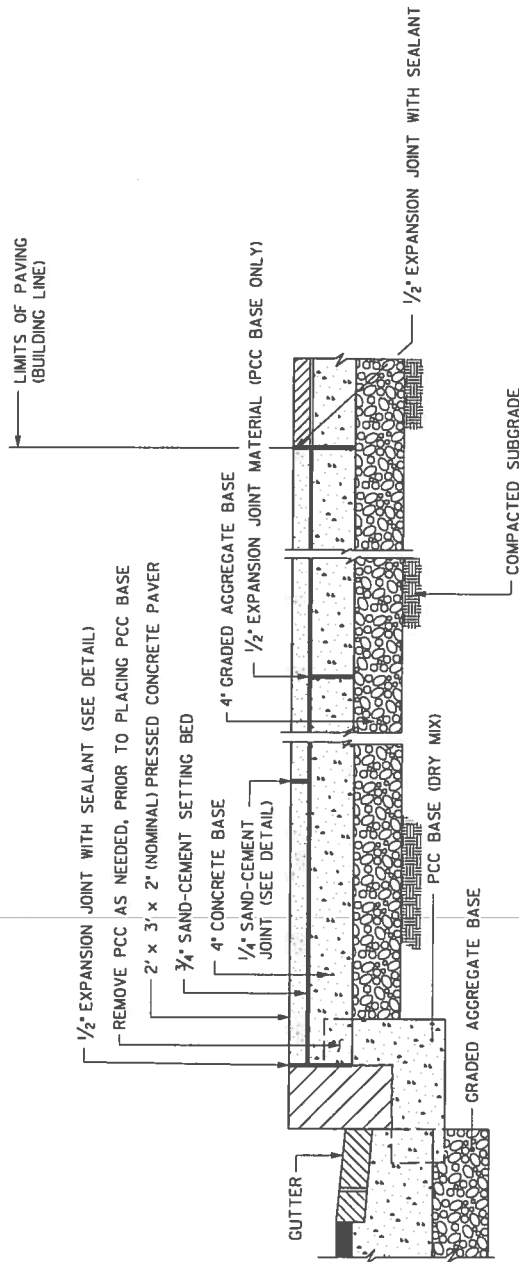
DWG. NO. 601.05

12" PCC BUS PAD TYPICAL SECTION

RECOMMENDED: *[Signature]*
DEPUTY CHIEF ENGINEER

APPROVED: *[Signature]*
CHIEF TRANSPORTATION ENGINEER

DATE	APPR.	REVIS.	ISSUED:	REFERENCE



PRESSED CONCRETE BLOCK PAVER SIDEWALK

1. ALL PRESSED CONCRETE PAVING BLOCKS SHALL HAVE A NON-SLIP SURFACE.
2. USE TRI-SECTED PATTERN, STARTING PERPENDICULAR AT CURB AND WORKING TOWARD BUILDING LINE.
3. SETTING BED SHALL BE SAND-CEMENT MIX, 2:1 BY VOLUME.
4. JOINTS SHALL BE SWEEPED WITH DRY SAND-CEMENT MIX, 2:1 BY VOLUME.
5. PAVING BLOCKS SHALL BE CUT TO FIT AROUND MANHOLES, VAULTS CATCH BASINS, CURBS, RAMPS, LIGHT POLES, KIOSKS AND FLAG POLES.
6. POURED CONCRETE SQUARE OR RECTANGULAR COLLARS AROUND SIDEWALK INTERRUPTIONS, USING AGGREGATE SIZE AND COLOR PER THE MANUFACTURER OF THE PRESSED CONCRETE PAVING BLOCKS, MAY BE USED SUBJECT TO APPROVAL BY THE ENGINEER.

- [illegible]

PRESSED CONCRETE BLOCK PAVER SIDEWALK

DISTRICT OF COLUMBIA
DEPARTMENT OF TRANSPORTATION

DWG. NO. 608.02



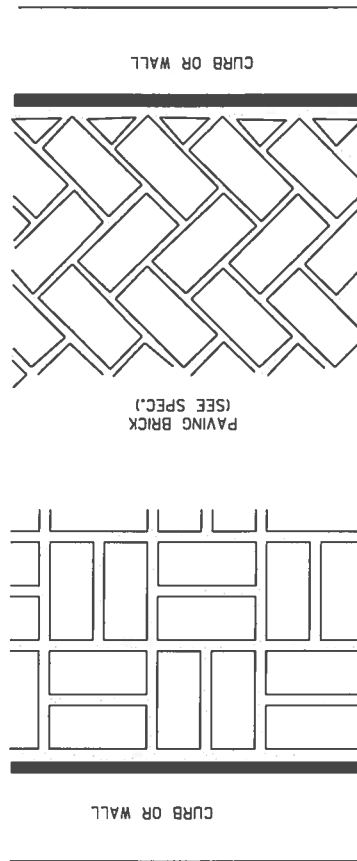
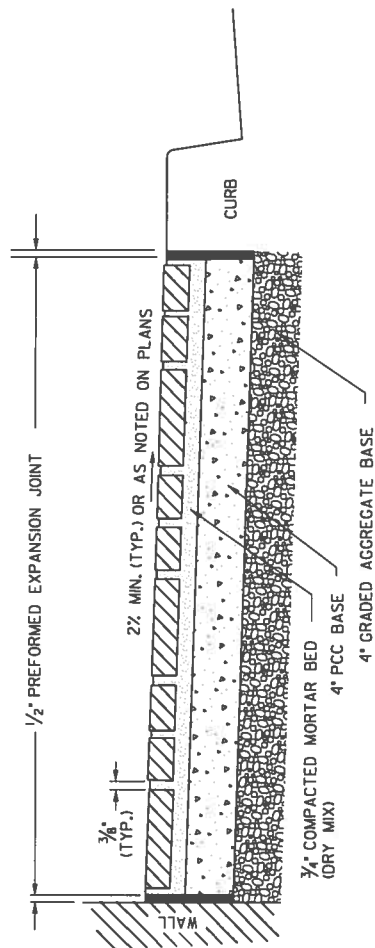
1. SEE CONTRACT PLANS FOR EXACT LOCATION OF WHEELCHAIR RAMPS.

ॐ

DISTRICT OF COLUMBIA
DEPARTMENT OF TRANSPORTATION

DWG. NO. 608.03

[illegible]



PATTERNS FOR BRICK PAVING

NOTE:

1. BRICK PATTERN SHALL BE AS SHOWN ON THE CONTRACT PLANS OR AS STIPULATED IN THE SPECIAL PROVISIONS.

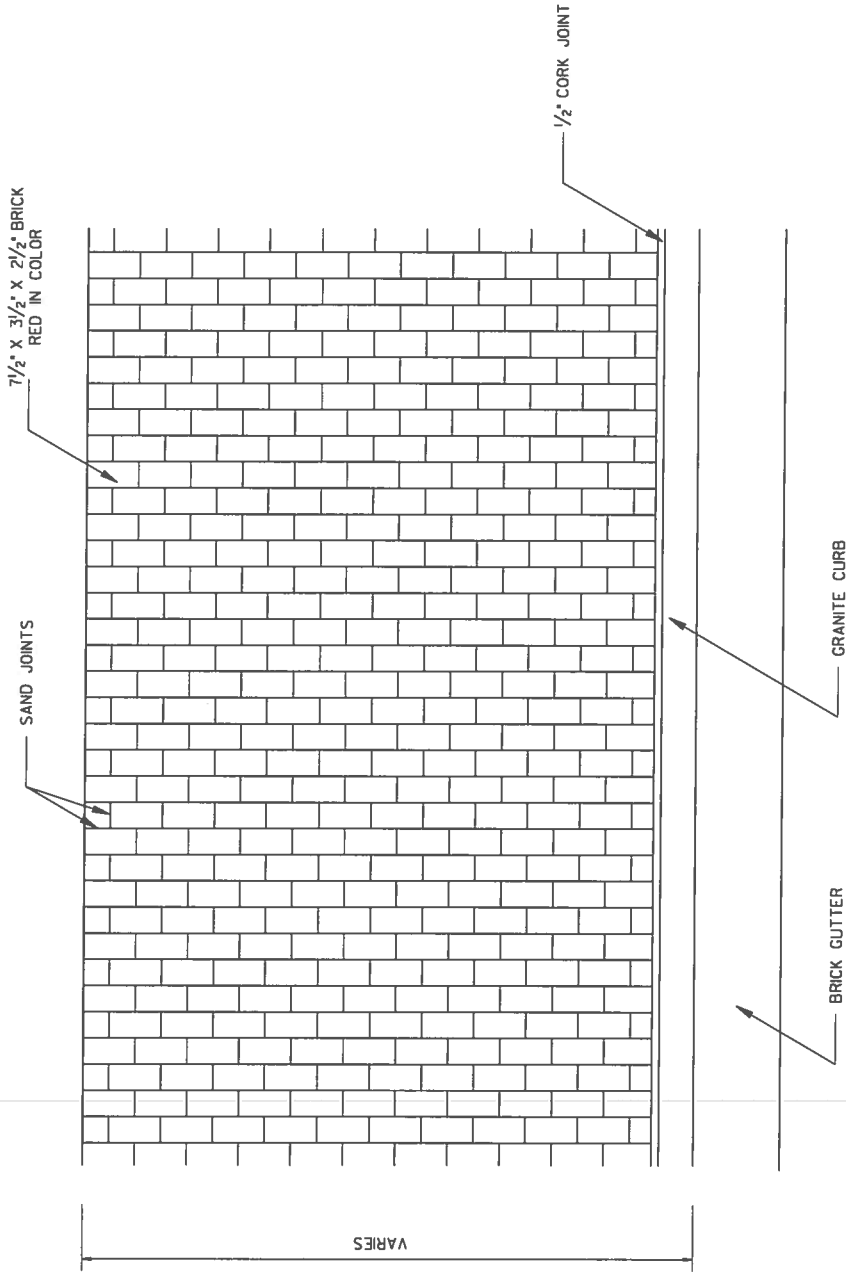
RECOMMENDED: [Signature]
DEPUTY CHIEF ENGINEER

APPROVED: K. H. H.
CHIEF TRANSPORTATION ENGINEER

BRICK ON 4" PCC BASE

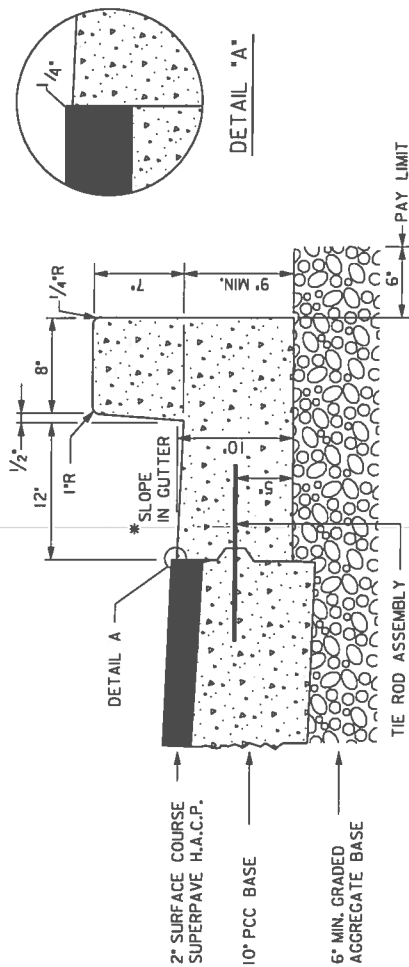
DISTRICT OF COLUMBIA
DEPARTMENT OF TRANSPORTATION

DWG. NO. 608.04

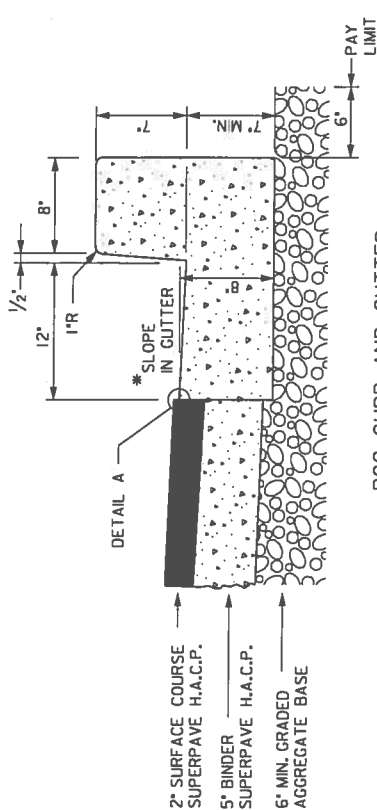


NOTE:
 1. NEW SIDEWALKS SHOULD BE BRICK ON CONCRETE IN RESIDENTIAL AND SPECIAL PURPOSE ZONED DISTRICTS, WITH A MINIMUM CONCRETE BASE OF 4 INCHES.
 2. REFER TO CHAPTER 31 IN DESIGN AND ENGINEERING MANUAL FOR MORE INFORMATION.

DISTRICT OF COLUMBIA DEPARTMENT OF TRANSPORTATION		DWG. NO. 608.05	
d.		PATTERNS FOR BRICK SIDEWALKS	
RECOMMENDED: DEPUTY CHIEF ENGINEER	APPROVED: CHIEF TRANSPORTATION ENGINEER	REFERENCE	
DATE APPR. REVISED ISSUED:	REFERENCE		



PCC CURB AND GUTTER
WITH CONCRETE PAVEMENT



PCC CURB AND GUTTER WITH FLEXIBLE PAVEMENT

NOTES:

1. TIE ROD ASSEMBLY TO BE INSTALLED AT 5 IN. OF THE INITIAL POUR.
2. #4 BARS SHALL NOT EXTEND THROUGH THE EXPANSION JOINT.
3. * LOW SIDE - 1 IN. PER FT. TOWARD CURB.
* HIGH SIDE - s_0 IN. PER FT. AWAY FROM CURB.
4. 6 IN. MIN. DEPTH GRADED AGGREGATE BASE APPLIES TO AREA BENEATH ROADWAY AND CURB AND GUTTER.

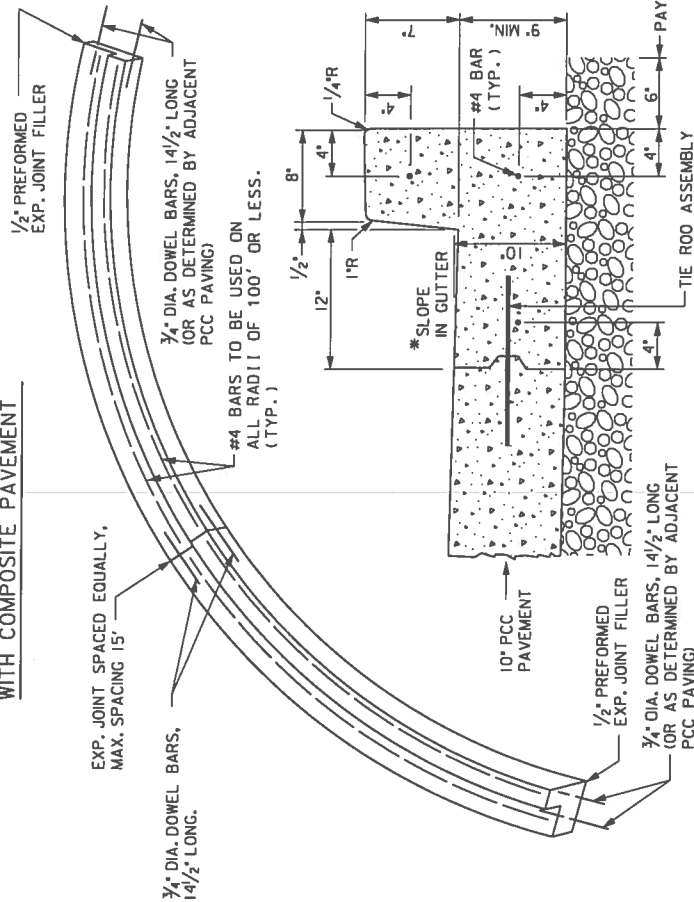
TYPES OF PCC CURB & GUTTER

DISTRICT OF COLUMBIA
DEPARTMENT OF TRANSPORTATION

DWG. NO. 609.01

609.01

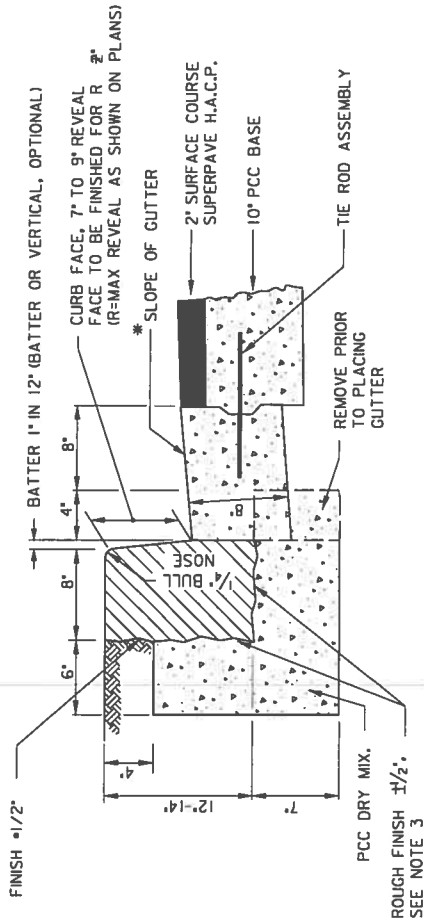
PCC CURB AND GUTTER WITH COMPOSITE PAVEMENT



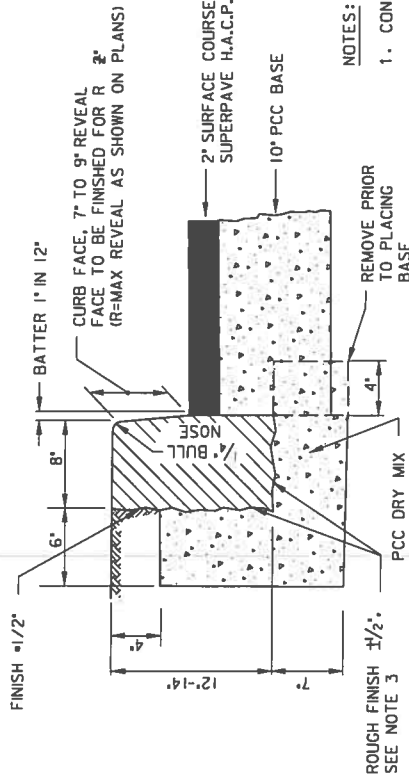
CIRCULAR REINFORCED PCC CURB AND GUTTER, RADII = 100' OR LESS

(SHOWN WITH CONCRETE PAVEMENT SECTION)

[illegible]



8' x 12' GRANITE CURB
(WITH PCC GUTTER)



8' x 12' GRANITE CURB
(WITHOUT PCC GUTTER)

NOTES:

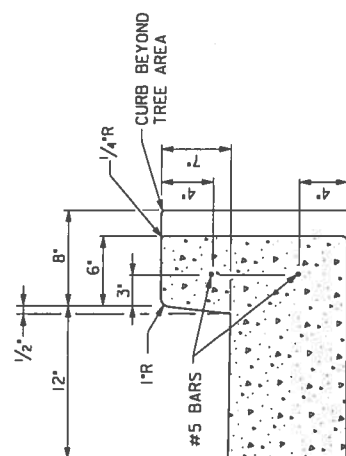
1. CONDITIONS AT BACK OF CURB VARY AND ARE AS SHOWN ON THE CONTRACT PLANS.
2. PCC DRY MIX SHALL BE PER DOT STANDARD SPECIFICATIONS, SECTION 801. IT SHALL MAINTAIN THE SAME TIME LIMITS AS PCC AND SHALL BE WATERED DOWN AFTER SETTING OF GRANITE CURB.
3. THE MINIMUM DEPTH TO CONCAVE SURFACE ON ROUGH FINISH SHALL BE 10 IN.
4. GRANITE CURBS ARE SHOWN WITH A COMPOSITE PAVEMENT SECTION.
5. * LOW SIDE - 1 IN. PER FT. TOWARD CURB
* HIGH SIDE - 5/8 IN. PER FT. AWAY FROM CURB
6. A 6 IN. MIN. LAYER OF GRADED AGGREGATE BASE SHALL BE PLACED BENEATH THE ROADWAY AND CURB AND GUTTER AND IS NOT SHOWN FOR CLARITY.

TYPES OF GRANITE CURBS

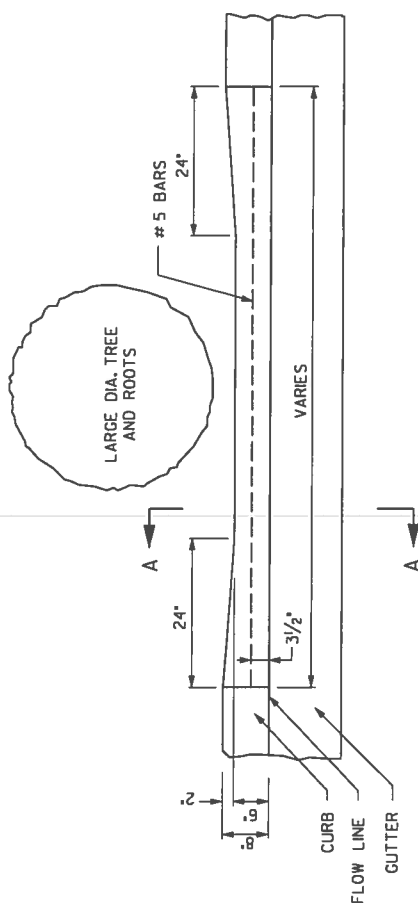
d. DISTRICT OF COLUMBIA
DEPARTMENT OF TRANSPORTATION

DWG. NO. 609.02

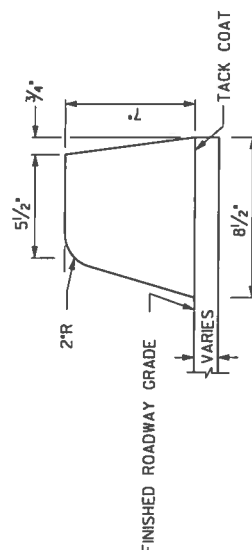
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SECTION A-A



STRAIGHT REINFORCED CONCRETE CURB - PLAN (WITH REDUCED WIDTH)



ASPHALT CURB

DISTRICT OF COLUMBIA
DEPARTMENT OF TRANSPORTATION

DWG. NO. 609.03

TYPES OF CURBS

RECOMMENDED: [Signature]
DEPUTY CHIEF ENGINEER

APPROVED: [Signature]
CHIEF TRANSPORTATION ENGINEER

[illegible]

PATTERNS FOR BRICK GUTTER

APPROVED: *[Signature]*
CHIEF TRANSPORTATION ENGINEER

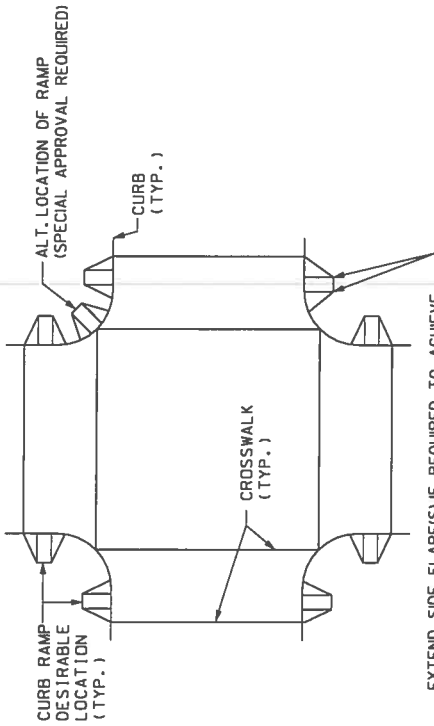
DATE	APPR.	
REVISED		
ISSUED:		
		REFERENCE



NOTES:

1. CONDITIONS AT BACK OF CURB VARY AND ARE AS SHOWN ON THE CONTRACT PLANS.
2. PCC DRY MIX SHALL BE PER DDOT STANDARD SPECIFICATIONS, SECTION 801. IT SHALL MAINTAIN THE SAME TIME LIMITS AS PCC AND SHALL BE WATERED DOWN AFTER SETTING OF GRANITE CURB.
3. THE MINIMUM DEPTH TO CONCAVE SURFACE ON ROUGH FINISH SHALL BE 10 IN.
4. GRANITE CURB IS SHOWN WITH A COMPOSITE PAVEMENT SECTION.
5. A 6 IN. MIN. LAYER OF GRADED AGGREGATE BASE SHALL BE PLACED BENEATH THE ROADWAY AND CURB AND GUTTER AND IS NOT SHOWN FOR CLARITY.

STRAIGHT SECTION

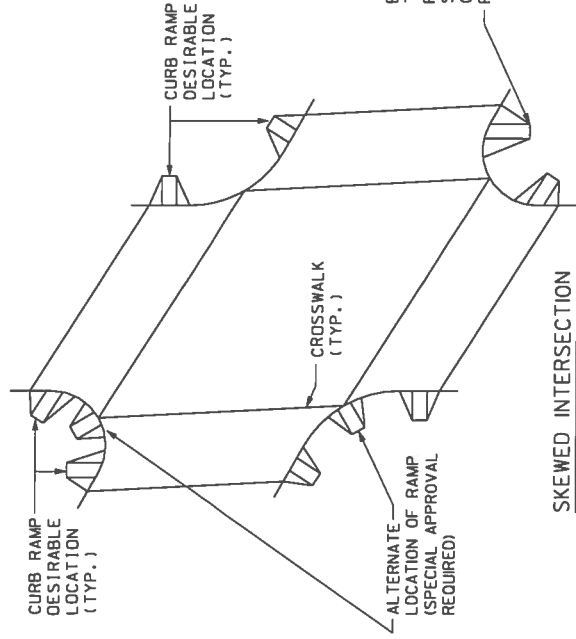


EXTEND SIDE FLARE(S) IF REQUIRED TO ACHIEVE MAXIMUM 12:1 SLOPE IN FLARE(S) (WHERE 12:1 LONGITUDINAL SLOPE IN CENTER OF RAMP AND 4'-0" CLEAR SIDEWALK SPACE IN BACK OF RAMP CANNOT BE OBTAINED).

RIGHT-ANGLE INTERSECTION

NOTES:

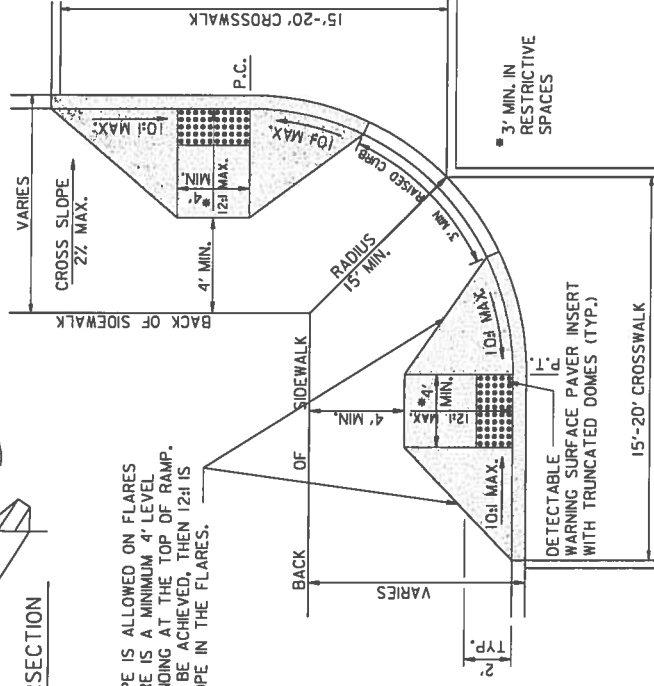
1. AT FOUR LEGGED INTERSECTIONS, IT IS REQUIRED TO CONSTRUCT TWO RAMPS, ONE FOR EACH DIRECTION OF CROSSING. IF THERE ARE SPACE LIMITATIONS THAT DO NOT PERMIT THE CONSTRUCTION OF TWO INDEPENDENT RAMPS, SUCH AS TELEPHONE POLES, FIRE HYDRANTS, STORM DRAIN INLETS, ETC., PROVISIONS SHOULD BE MADE TO RELOCATE THE OBSTRUCTION. IF THE 12:1 LONGITUDINAL SLOPE CANNOT BE ACHIEVED WHERE RAMPS ARE SHOWN, SPECIAL APPROVAL SHALL BE OBTAINED TO CONSTRUCT ONE RAMP AT THE CORNER AND MODIFY THE CROSSWALKS. SEE DRAWINGS 609.06 AND 609.07 FOR DETAILS.
2. RAMP LOCATION SHALL BE GOVERNED BY CROSSWALK WIDTH, 15'-20' AS DIRECTED. ALL RAMPS, INCLUDING SIDE FLARES, SHALL BE LOCATED WITHIN A CROSSWALK. ONE SIDE FLARE SHALL ALIGN WITH THE BACK EDGE LINE OF THE CROSSWALK.
3. FOR SKEWED INTERSECTION, ACUTE CORNER SHALL DETERMINE THE LOCATION OF LIGHT POLES, RAMPS AND CROSSWALKS.
4. ALL RAMPS SHALL CONFORM TO THE LATEST AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES (ADAAG) CRITERIA.
5. DIMENSIONS SHOWN ARE FOR NEW CONSTRUCTION. FOR ALTERATIONS WHEN THESE DIMENSIONS ARE IMPRACTICAL, REVIEW ADAAG FOR LESS STRICT DIMENSIONS.
6. INSTALL DETECTABLE WARNING SURFACE PAVERS WITH TRUNCATED DOMES FOR A DISTANCE OF 24" FROM THE BACK OF THE CURB AS SHOWN.
7. THE SURFACE OF THE RAMP SHALL BE BROOM FINISHED (STEEL BRISTLE).
8. RAMP SHALL BE CONSTRUCTED WITH PCC SIDEWALK CONCRETE (NO DARKENING AGENTS).
9. ANY LIGHT POLE FOUNDATION SHALL BE CONSTRUCTED INDEPENDENTLY OF RAMP.
10. DESIGN STORM DRAIN SYSTEMS TO SHED WATER AWAY FROM RAMPS.
11. FINAL LOCATION OF RAMP WILL BE DECIDED BY THE ENGINEER ON SITE.



EXTEND SIDE FLARE(S) IF REQUIRED TO ACHIEVE MAXIMUM 12:1 SLOPE IN FLARE(S) (WHERE 12:1 LONGITUDINAL SLOPE IN CENTER OF RAMP AND 4'-0" CLEAR SIDEWALK SPACE IN BACK OF RAMP CANNOT BE OBTAINED).

SKEWED INTERSECTION

10:1 MAX. SLOPE IS ALLOWED ON FLARES ONLY IF THERE IS A MINIMUM 4' LEVEL (2% MAX.) LANDING AT THE TOP OF RAMP. IF 4' CANNOT BE ACHIEVED, THEN 12:1 IS THE MAX. SLOPE IN THE FLARES.



DETAIL: WHEELCHAIR - BICYCLE RAMP(S)
DESIRABLE LOCATION (TYP.)

WHEELCHAIR - BICYCLE RAMPS LOCATIONS

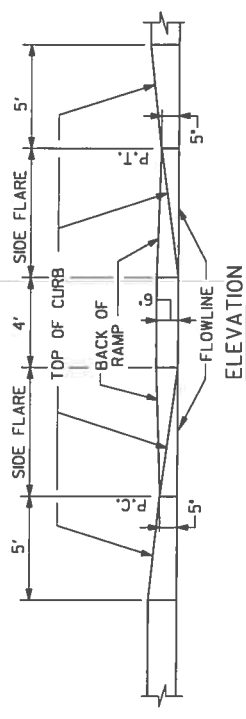
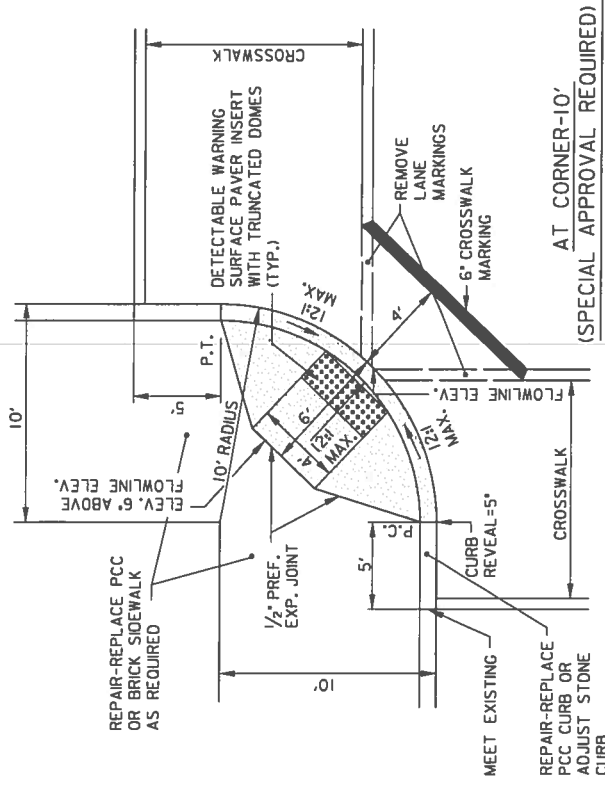
DISTRICT OF COLUMBIA
DEPARTMENT OF TRANSPORTATION

RECOMMENDED:
DEPUTY CHIEF ENGINEER

APPROVED:
CHIEF TRANSPORTATION ENGINEER

DATE	APPR.	ISSUED:	REFERENCE

DWG. NO. 609.05



NOTES:

1. FOR SKEWED INTERSECTION, ACUTE CORNER SHALL DETERMINE THE LOCATION OF LIGHT POLES, RAMPS AND CROSSWALKS.
2. ALL RAMPS SHALL CONFORM TO THE LATEST AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES (ADAAG) CRITERIA.
3. INSTALL DETECTABLE WARNING SURFACE PAVEMENTS WITH TRUNCATED DOMES FOR A DISTANCE OF 24" FROM THE BACK OF THE CURB AS SHOWN.
4. THE SURFACE OF THE RAMP SHALL BE BROOM FINISHED (STEEL BRISTLE).
5. RAMP SHALL BE CONSTRUCTED WITH PCC SIDEWALK CONCRETE (NO DARKENING AGENTS).
6. ANY LIGHT POLE FOUNDATION SHALL BE CONSTRUCTED INDEPENDENTLY OF RAMP.
7. DESIGN STORM DRAIN SYSTEMS TO SHED WATER AWAY FROM RAMPS.
8. FOR WIDTHS OF SIDEWALK & ANGLES OF INTERSECTION DIFFER FROM THAT SHOWN, THE ENGINEER WILL MODIFY RAMP DESIGN ACCORDINGLY SO THAT SLOPE REQUIREMENTS ARE MET.
9. DETAILS SHOWN FOR INSTALLATION OF RAMPS IN EXIST. CONSTRUCTION TO BE USED ONLY WHEN 2 RAMPS AT CORNER CANNOT BE CONSTRUCTED IN ACCORDANCE WITH ADAAG CRITERIA.
10. GUTTER, IF ANY, NOT SHOWN. REPAIR OF GUTTER, IF REQUIRED, SHALL BE DONE UNDER THE APPROPRIATE PAY ITEM.
11. THE FINAL LOCATION OF RAMP WILL BE DECIDED BY THE ENGINEER ON SITE.

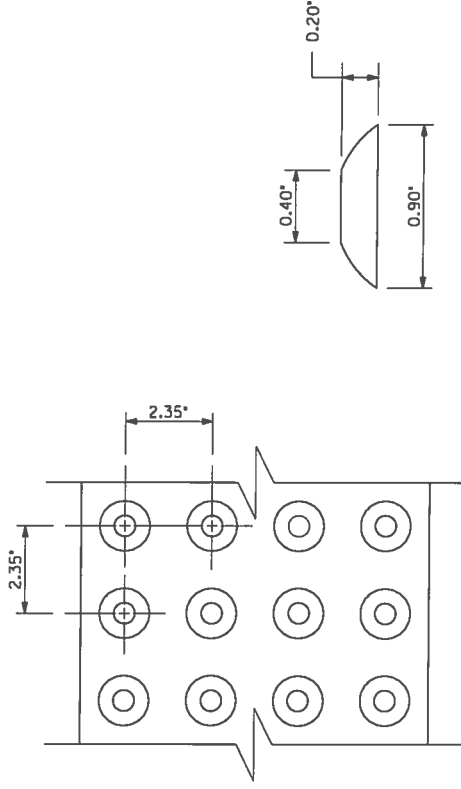
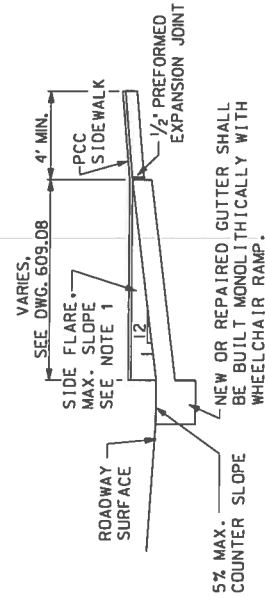
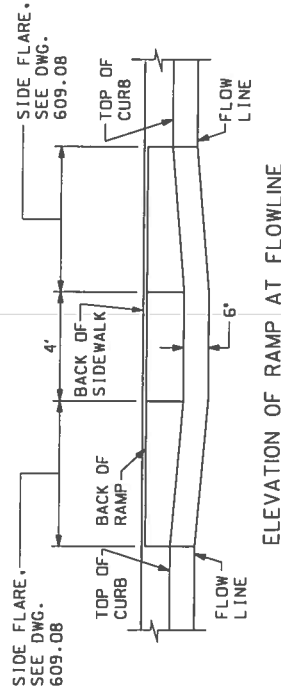
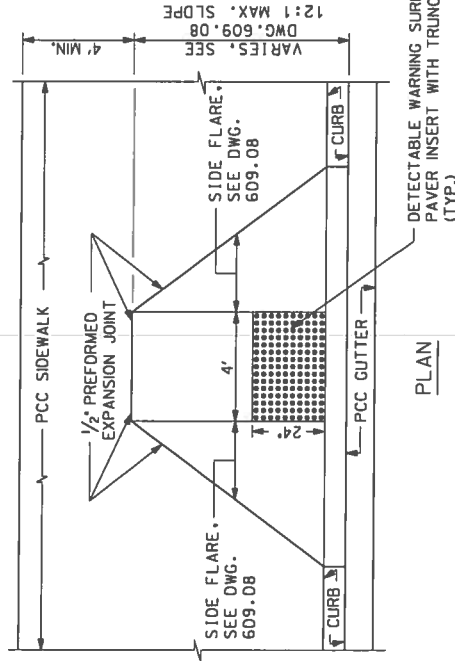
DATE	APPR.	ISSUED:	REFERENCE	RECOMMENDED:
				DEPUTY CHIEF ENGINEER
REVISED				APPROVED:
				CHIEF TRANSPORTATION ENGINEER

WHEELCHAIR-BICYCLE RAMPS WITHIN CORNER RADIUS

d. DISTRICT OF COLUMBIA
DEPARTMENT OF TRANSPORTATION

NOTES:

1. EITHER (1) SIDEWALK SLOPE BEHIND RAMP SHALL BE REDUCED, BUT NOT LESS THAN 0.5%, OR (2) IF RIGHT-OF-WAY AND PHYSICAL CONDITIONS PERMIT, SIDEWALK SHALL BE EXTENDED, SO THAT MAXIMUM SLOPE OF 12:1 ALONG CENTERLINE OF RAMP IS ACHIEVED. IF 12:1 SLOPE CANNOT BE ACHIEVED BY (1) OR (2) ABOVE, THEN MAXIMUM SLOPE IN AT LEAST ONE SIDE FLARE SHALL BE 12:1.



DOMES SECTION

DOMES SECTION

DETECTABLE WARNING SURFACE PAVEMENT/TRUNCATED DOME NOTES:


1. DETECTABLE WARNING SURFACE PAVEMENT/TRUNCATED DOME INSERT SHALL BE INSTALLED 24" FROM THE BACK OF THE CURB AS SHOWN.
2. DETECTABLE WARNING SURFACE PAVEMENT SHALL CONSIST OF A SURFACE OF TRUNCATED DOMES ALIGNED ON A SQUARE GRID IN THE PREDOMINANT DIRECTION OF TRAVEL TO PERMIT WHEELS TO ROLL BETWEEN DOMES.
3. DETECTABLE WARNING SURFACE PAVEMENT/TRUNCATED DOME INSERT SHALL CONTRAST VISUALLY WITH ADJACENT WALKING SURFACES.

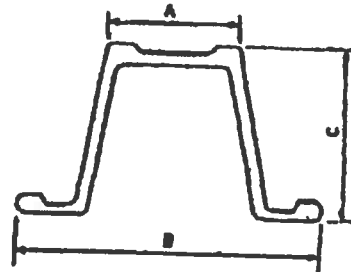
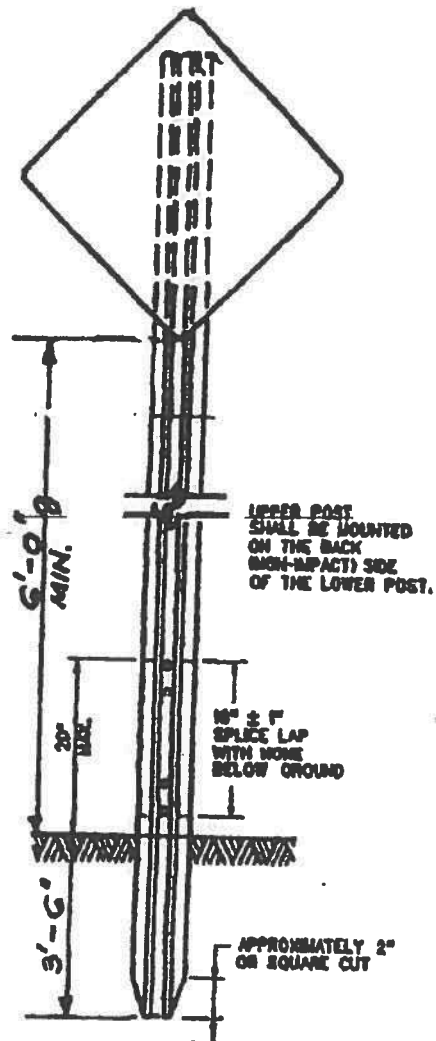
**WHEELCHAIR-BICYCLE RAMPS
DETAILS**

d. DISTRICT OF COLUMBIA
DEPARTMENT OF TRANSPORTATION

DWG. NO. 609.07

DATE	APPROVED	ISSUED	REFERENCE	RECOMMENDED
APPROVED	DEPUTY CHIEF ENGINEER			
APPROVED	CHIEF TRANSPORTATION ENGINEER			

 $\frac{1}{2}$ " HIGH
STRENGTH BOLT
12 REQUIRED AT EACH
END OF SPLICE



3.00 LB./LF.

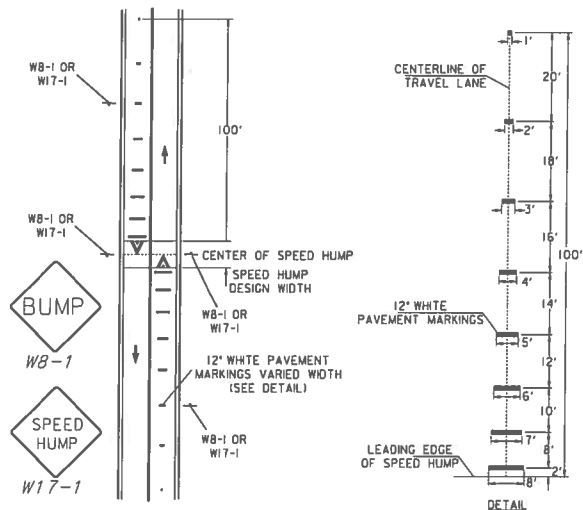
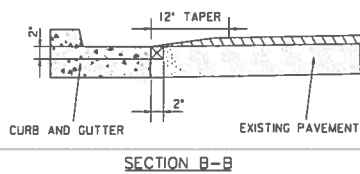
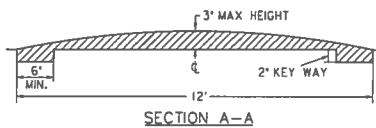
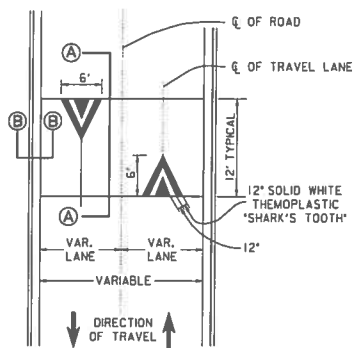
	MIN.	MAX.
A.	1 $\frac{1}{4}$ "	1 $\frac{1}{2}$ "
B.	3 $\frac{1}{8}$ "	3 $\frac{1}{4}$ "
C.	$\frac{1}{2}$ "	1 $\frac{1}{4}$ "

LENGTH IS VARIABLE
WEIGHT IS PER LINEAR FOOT

POST SECTION DIMENSIONS

SPLICING DETAIL

STEEL DRIVE POST DETAIL



NOTE:

1. SPEED BUMPS - NARROW MOUNTABLE OBSTRUCTIONS INSTALLED ON THE PAVEMENT SURFACE, ACROSS THE TRAVELED LANES, AND INTENDED TO CAUSE VEHICLES TO SLOW. SPEED BUMPS ARE USUALLY LESS THAN 14 INCHES WIDE AND 4 INCHES HIGH.
2. SPEED HUMPS - THEY ARE SIMILAR TO SPEED BUMPS, BUT UTILIZE LARGER VERTICAL RADIUS THAT RESULT IN WIDER WIDTHS AND A MORE GENTLE CROSSING BY VEHICLES.
3. SPEED TABLES - WIDE MOUNTABLE OBSTRUCTIONS INSTALLED ON THE PAVEMENT SURFACE ACROSS THE TRAVELED LANES, AND INTENDED TO CAUSE VEHICLES TO SLOW. THEY ARE SIMILAR TO SPEED HUMPS, EXCEPT FOR THE FLAT-TOPPED SECTION LOCATED BETWEEN THE APPROACH AND FAR EDGES. SPEED TABLES ARE GENERALLY WIDER THAN SPEED HUMPS AND ARE MORE GENTLE ON VEHICLES.
4. SPEED BUMPS/HUMPS/TABLES SHOULD NOT BE CONSIDERED ON: EMERGENCY AND EVACUATION ROUTES; ROADWAYS WITH GRADES OF 7% OR MORE; ARTERIALS OR COLLECTOR STREETS AND THROUGH TRUCK ROUTES; AT DRIVEWAY, ENTRANCE AND/OR ALLEY.
5. SPEED BUMPS/HUMPS/TABLES SHOULD NOT BE PLACED WITHIN 150 FEET OF AN UNSIGNALIZED INTERSECTION OR 250 FEET OF A SIGNALIZED INTERSECTION.
6. SPEED BUMPS/HUMPS/TABLES ARE TYPICALLY PLACED 250-350 FEET APART.
7. REFER TO "DESIGN GUIDELINES FOR TRAFFIC CALMING MEASURES FOR RESIDENTIAL STREETS IN THE DISTRICT OF COLUMBIA (JUNE 2005)" FOR ADDITIONAL INFORMATION.

APPROVED: ATTORNEY GENERAL OF THE DISTRICT OF COLUMBIA
 1/1/2009, 10:00 AM, 10:00 AM, 10:00 AM

DATE	APPR.	RECOMMENDED
REVIS		DEPUTY CHIEF ENGINEER
ISSUED		APPROVED: <i>Kent Perry</i>
REFERENCE		CHIEF TRANSPORTATION ENGINEER

TRAFFIC CALMING
 SPEED HUMP
 STANDARD DRAWINGS

d. DISTRICT OF COLUMBIA
 DEPARTMENT OF TRANSPORTATION

DWG. NO. 616.28

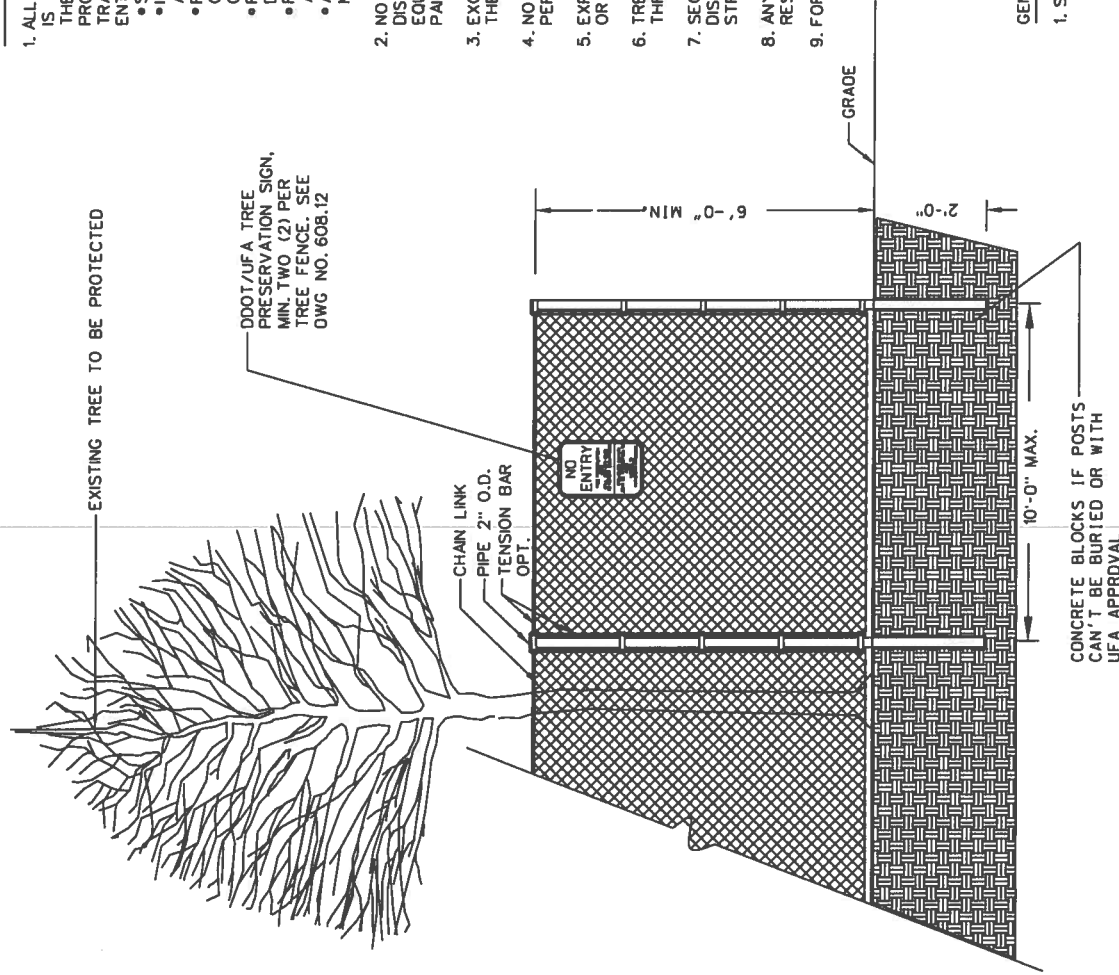
DWG. NO. 616.29

TREE PROTECTION NOTES:

- ALL EXISTING STREET TREES, TO REMAIN WITHIN A WORK ZONE UNTIL A PROJECT IS COMPLETED, REQUIRE THE FOLLOWING AS TREE PROTECTION. IF FOR ANY REASON THE SCOPE OF THE PROJECT REQUIRES WORK TO BE PERFORMED WITHIN THE FENCED PROTECTION ZONE, THE PERMIT HOLDER MUST CONTACT THE DISTRICT DEPARTMENT OF TRANSPORTATION'S URBAN FORESTRY ADMINISTRATION (UFA) AT 202-671-5133 BEFORE ENTERING.
 - SIX (6) FOOT TALL CHAIN LINK FENCING ON ALL SIDES.
 - INSTALL FENCING PRIOR TO AND MAINTAIN THROUGHOUT CONSTRUCTION, REMOVING ONLY AT THE END OF THE PROJECT.
 - FENCING SHALL PROTECT AN AREA NO SMALLER THAN FOUR (4) FEET BY NINE (9) FEET, CENTERED ON THE TREE, OR ENCLOSE MULTIPLE STREET TREES WHEN IN A CONTINUOUS, OPEN TREE PLANTING SPACE AND SITE ACCESS ALLOWS.
 - FENCING SHALL HAVE VERTICAL AND HORIZONTAL SUPPORT RAILINGS TO DECREASE FLEXIBILITY AND PREVENT SAGGING.
 - FENCE POSTS SHALL BE ANCHORED IN THE GROUND TO PREVENT MOVEMENT AND PROVIDE A SECURE BARRIER.
 - A MINIMUM OF TWO (2) DDOT/UFA STANDARD TREE PRESERVATION SIGNS SHALL BE MOUNTED ON THE FENCE OF EACH ENCLOSED TREE PROTECTION AREA.
- NO INSTALLATION OF SILT FENCE/SUPER SILT FENCE, TRENCHING, ALTERATION OR DISTURBANCE TO EXISTING GRADE; STAGING/STORAGE OF CONSTRUCTION MATERIALS, EQUIPMENT, SOIL, OR DEBRIS; DISPOSAL OF ANY MATERIALS SUCH AS CONCRETE, GAS, OIL, PAINT, OR BLACKTOP IS ALLOWED WITHIN THE FENCED TREE PROTECTION ZONE.
- EXCAVATIONS WITHIN THE DRIP LINE SHALL PROCEED WITH CARE BY USE OF HAND TOOLS. THE DRIFLINE IS DEFINED AS THE GROUND AREA UNDER THE CANOPY OF THE TREE.
- NO ROOTS LARGER THAN TWO (2) INCHES IN DIAMETER ARE TO BE CUT WITHOUT UFA PERMISSION.
- EXPOSED ROOTS TWO (2) INCHES AND LARGER IN DIAMETER SHALL BE WRAPPED IN BURLAP OR OTHER APPROVED MATERIAL AND KEPT MOIST AT ALL TIMES.
- TREES THAT ARE PROTECTED ARE TO BE WATERED EVERY TEN (10) DAYS FROM APRIL THROUGH SEPTEMBER.
- SECTION 608.07 - TREE PROTECTION AND 608.08 - TREE ROOT PROTECTION OF THE 2013 DISTRICT DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAYS AND STRUCTURES WILL APPLY SHOULD ANY DAMAGE OCCUR TO THE EXISTING STREET TREES.
- ANY FINES RELATED TO DAMAGE TO A STREET TREE ON A JOB SITE SHALL BE THE RESPONSIBILITY OF THE PERMIT HOLDER.
- FOR ANY QUESTIONS, CALL DDOT URBAN FORESTRY ADMINISTRATION AT 202-671-5133.

GENERAL NOTES:

- SEE TREE PROTECTION FENCE - PLAN, DWG. NO. 608.11



**TREE PROTECTION FENCE -
ELEVATION**

d.
DISTRICT OF COLUMBIA
DEPARTMENT OF TRANSPORTATION

DWG. NO. 608.10

RECOMMENDED:		PROJECT MANAGER	
DATE	APPR.	APPROVED:	
REVISED			
ISSUED:			
REFERENCE		CHIEF TRANSPORTATION ENGINEER	

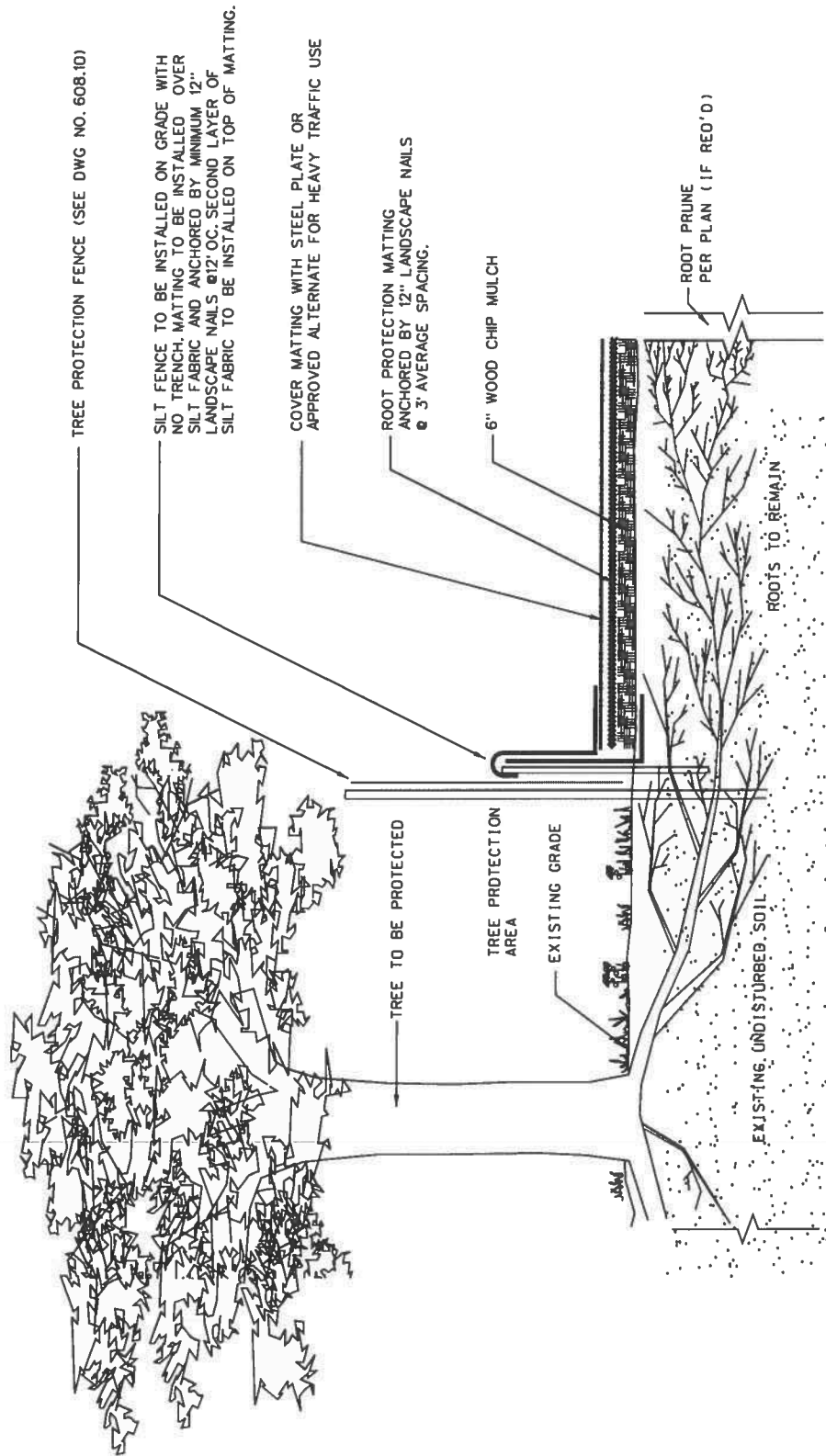


2. SEE TREE PROTECTION FENCE-ELEVATION, DWG. NO. 608.10 FOR TREE PROTECTION NOTES.

TREE PROTECTION FENCE -
PLAN VIEW

DISTRICT OF COLUMBIA
DEPARTMENT OF TRANSPORTATION

DWG. NO. 608.11



TREE PROTECTION FENCE (SEE DWG NO. 608.10)

SILT FENCE TO BE INSTALLED ON GRADE WITH NO TRENCH. MATTING TO BE INSTALLED OVER SILT FABRIC AND ANCHORED BY MINIMUM 12" LANDSCAPE NAILS @ 12" OC. SECOND LAYER OF SILT FABRIC TO BE INSTALLED ON TOP OF MATTING.

COVER MATTING WITH STEEL PLATE OR APPROVED ALTERNATE FOR HEAVY TRAFFIC USE

ROOT PROTECTION MATTING ANCHORED BY 12" LANDSCAPE NAILS @ 3' AVERAGE SPACING.

6" WOOD CHIP MULCH

ROOT PRUNE PER PLAN (IF REQ'D)

ROOTS TO REMAIN

NOTES:

1. MATTING MATERIAL SHALL BE DOUBLE SIDED GEOCOMPOSITE, GEONET CORE WITH NON-WOVEN COVERING (SUCH AS TENSAR ROADRAIN RD7) OR APPROVED EQUIVALENT.
2. ROOT PROTECTION MATTING SHALL BE INSTALLED BY A CERTIFIED ARBORIST.
3. TO BE USED FOR DESIGNATED TEMPORARY CONSTRUCTION ACCESS AND STOCKPILE AREAS.
4. MATTING SHALL BE PLACED ON 6" WOOD CHIP MULCH UNLESS OTHERWISE DIRECTED.
5. FOR HEAVY TRAFFIC AREAS, MATTING SHALL BE COVERED WITH STEEL PLATES.

				RECOMMENDED:	PROJECT MANAGER
DATE	APPR.			APPROVED:	
REVISED					
ISSUED:				REFERENCE	CHIEF TRANSPORTATION ENGINEER

TREE ROOT PROTECTION W/SILT FENCE

d.
DISTRICT OF COLUMBIA
DEPARTMENT OF TRANSPORTATION

DWG. NO. 608.13